

**1 SUMMARY OF APPLICATION DETAILS**

Ref: 19/03628/FUL  
 Location: 5 Croham Valley Road  
 Ward: Selsdon & Ballards  
 Description: Demolition of the existing property and erection of 6 houses (3 houses fronting Croham Valley Road and 3 houses fronting Ballards Farm Close), gardens, car parking, new accesses, refuse and recycling.  
 Drawing Nos: 31-P2-4H, 31-P2-5F, 31-P2-6C, 31-P2-7E, 31-P2-8C, 31-P2-9C, 31-P2-11C, 31-P2-14B, 31-P2-15A, 31-P2-16B, 31-P2-17B  
 Applicant: Sterling Rose  
 Case Officer: Pete Smith

	<b>3 bed</b>	<b>4 bed</b>
<b>Existing houses</b>	1	
<b>Proposed Houses</b>	3	3

All units are proposed for private sale

<b>Number of car parking spaces</b>	<b>Number of cycle parking spaces</b>
4	12

1.1 This application is being reported to Planning Committee as objections above the Committee Consideration Criteria have been received and as a consequence of referral requests from Councillor Michael Neal and the Croham Valley Residents Association.

**2 RECOMMENDATION**

2.1 That the Committee resolve to GRANT planning permission subject to the completion of a S.106 Agreement to secure the following heads of terms:

- A financial contribution of £6,000 towards highway management measures and the delivery of sustainable transport initiatives in and around Croham Valley Road and neighbouring streets.
- The costs associated with the removal and replanting of street trees (including on-going maintenance) to facilitate the formation of the crossover

- 2.2 That the Director of Planning and Strategic Transport has delegated authority to negotiate the legal agreement indicated above.
- 2.3 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

### **Conditions**

- 1) Development to be carried out in accordance with the approved drawings and reports except where specified by conditions.
- 2) Materials to be submitted with samples.
- 3) Details of refuse storage and cycle storage enclosures
- 4) Landscaping scheme to be submitted including hard/soft landscaping, retaining walls, the retention of hedging between 5 and 7 Croham Valley Road, boundary treatments and SUDs techniques.
- 5) Details of groundwater flooding mitigation to be submitted and agreed
- 6) Construction Logistics Plan and Environmental Management Plan to be submitted for approval.
- 7) No occupation to take place until such time as the access onto Croham Valley Road and minor changes to the existing Ballards Farm Road access, on site car parking, electric charging points, refuse and cycle storage have been provided as specified in the application and subsequently approved by condition
- 8) Removal of permitted development rights for enlargements and outbuildings
- 9) Side windows to be obscurely glazed
- 10) No additional windows in the flank elevations
- 11) 19% reduction in carbon emissions
- 12) Water usage restricted to 110 litres per person per day
- 13) Commencement of development within three years of consent being granted
- 14) Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

### **Informatives**

- 1) CIL liability
  - 2) Code of Practice for Construction Sites
  - 3) Ecological Informative
  - 4) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport
- 2.4 That the Planning Committee confirms that adequate provision has been made, by the imposition of conditions, for the preservation or planting of trees as required by Section 197 of the Town and Country Planning Act 1990.

### 3 PROPOSAL AND LOCATION DETAILS

#### Proposal

3.1 The proposal comprises the following:

- Erection of a three storey building with accommodation in the roof-space fronting onto Croham Valley Road, comprising 3x4 bedroom (7 person) houses,
- Erection of a two storey building with accommodation in the roof-space, fronting Ballards Farm Road, comprising 3x3 bedroom (4 person) houses,
- New vehicular access onto Croham Valley Road and utilisation of existing access onto Ballards Farm Road.
- Parking for four vehicles; two spaces accessed from Ballards Farm Road and two from Croham Valley Road
- Associated cycle storage, excavation, retaining walls and soft landscaping.

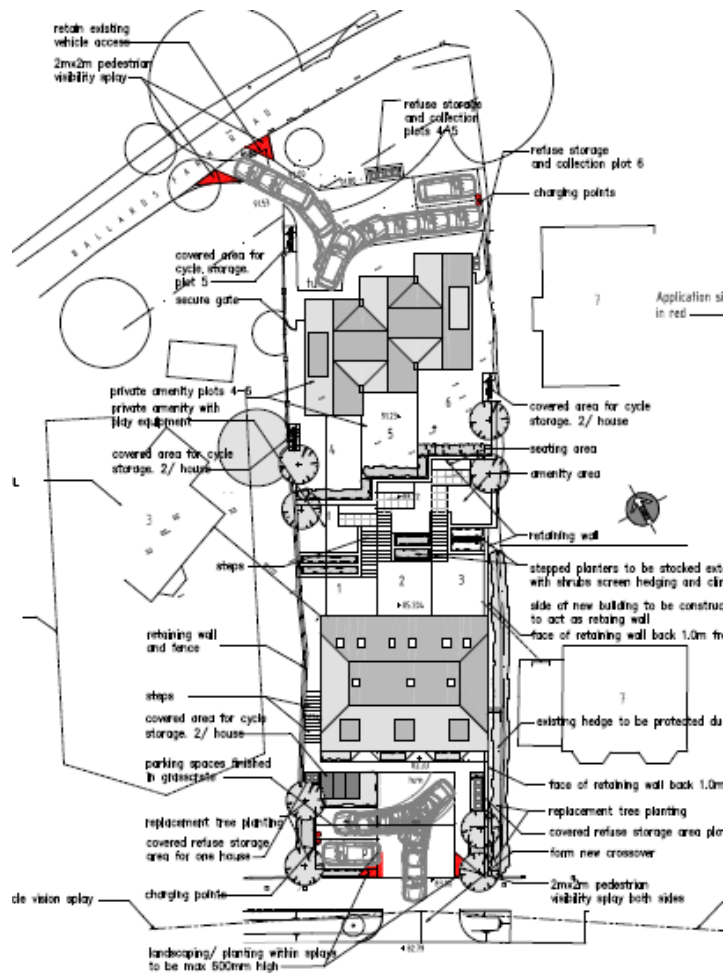


Figure 1 Proposed Site Layout

- 3.2 Amended drawings have been received during the course of the application, amending the appearance and materiality of the proposed building, providing additional sections through the site and reducing on site car parking to ensure that cars are able to turn on site and to enter and exit onto Ballards Farm Road in forward gear. Neighbours were re-notified in November 2019. A further amendment was made to the proposed retaining wall (between 5 and 7 Croham Valley Road) which has been set back by 1 metre (off the boundary) to ensure the retention of an existing hedged boundary condition. This amendment did not require re-notification but the most relevant party (the adjoining neighbour) was notified.

### Site and Surroundings

- 3.3 The site is currently occupied by a traditional two storey detached dwelling located towards the northern (highest) part of the site. It is angled within the plot, with main windows facing onto the garden attached to 3 Croham Valley Road. The plot and surrounding area is steeply sloping and falls significantly in height from north to south – down towards Croham Valley Road. The main entrance and vehicular access to the existing property is off Ballards Farm Road, which is a relatively narrow un-adopted lane with no pedestrian footways, although there is an existing pedestrian access – leading up to the property off Croham Valley Road.



*Figure 2 View from Croham Valley Road – looking north*

- 3.4 To the west is a two storey detached property situated to the top of the slope and accessed off Ballards Farm Road (3 Croham Valley Road). This property is the subject of separate planning application which is pending consideration (LBC Ref 18/06067/FUL) for the demolition of the existing house and erection of a five storey

building (two storey fronting Ballards Farm Road with basement levels fronting onto Croham Valley Road) comprising 7 flats with creation of new access off Croham Valley Road, 7 car parking spaces, refuse storage and landscaping.

- 3.5 To the east are two residential properties; 7 Croham Valley Road which is a two storey detached property (with rooms in the roof-space) which was granted planning permission in March 2015 on appeal (LBC Ref 14/05093/P) and 7 Ballards Farm Road, which is a two storey detached property.
- 3.6 The application site falls within a surface water critical drainage area with a low risk of surface water flooding and potential for groundwater flooding to occur at the surface. Land to the north of the site is a Site of Nature Conservation Importance and 7 Ballards Farm Road has trees protected by way of a tree preservation order TPO (No.8 1997) – although this tree is not located adjacent to the application site.
- 3.7 The site has a PTAL of 2. Footways are present on both sides of Croham Valley Road (with the highway having a 30 mph speed limit). The road is also part of the local cycle network and connects to South Croydon. Moreover, Croham Valley Road links (via Farley Road) to Addington Road which is a further cycle route connecting Sanderstead with Gravel Hill and beyond. There are two bus stops close by (serving the 64 and 433 bus routes).

### **Planning History**

- 3.8 The following applications are of relevance to the consideration of this application:
- 19/00281/FUL: Demolition of the existing dwelling and erection of nine residential units (six residential apartments and three houses) with associated refuse, recycling, cycle storage and car parking, and new vehicular crossover onto Croham Valley Road

**Permission refused (18/04/2019) on the following grounds:**

- 1) By reason of its mass, depth and height, the proposed development would have an unduly overbearing impact on 7 Croham Valley Road, causing an unacceptable loss of outlook and feeling of enclosure, particularly from the rear garden. The proposal is therefore contrary to the requirements of Policy DM10 of the Croydon Local Plan (2018), Suburban Design Guide Supplementary Planning Document (2019) and Policy 7.6 of the London Plan 2015 (consolidated with amendments since 2011).
- 2) The development would result in sub-standard accommodation for future occupiers by reason of poor access to daylight/sunlight and outlook and would provide communal amenity space of inadequate quality by reason of its scale, layout and topography. The proposal is therefore contrary to the Policies 3.5 and 7.4 of the London Plan 2015 (consolidated with alterations since 2011) Policy DM10 of the Croydon Local Plan (2018) and the London Housing SPG.

- 3) The development would create a hazard to pedestrians, cyclists and vehicular traffic using the highways by reason of inadequate visibility splays and unsafe access and parking arrangements and would thereby conflict with Paragraph 109 of the NPPF, Policies DM29 and DM30 of the Croydon Local Plan (2018) and Policies 6.3 and 6.12 of the London Plan 2015 (consolidated with amendments since 2011).

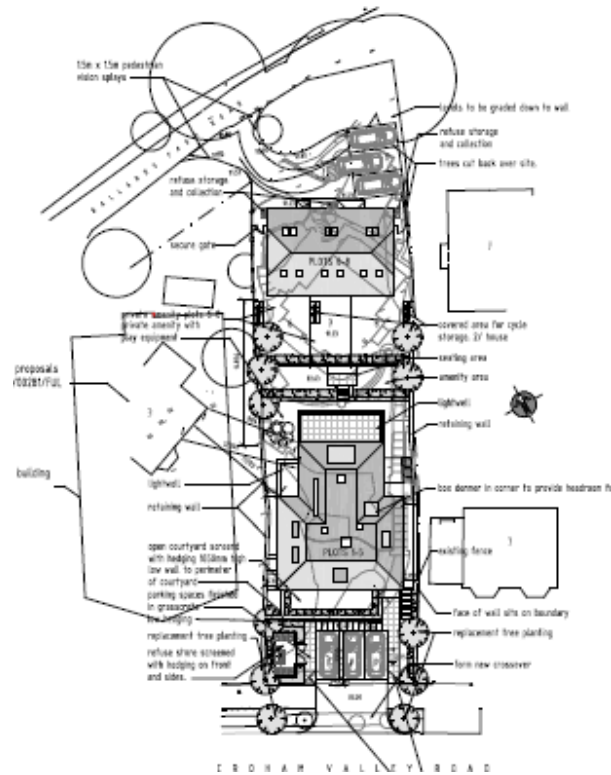


Figure 3 Previously Refused Site Layout

3.9 The current application seeks to overcome these previous reasons for refusal through a reduction in the number of units proposed, a greater focus towards houses (rather than flats), a reduced footprint (specifically the proposed houses fronting onto Croham Valley Road) and a reduced impact on the garden amenities of the neighbouring occupiers – specifically 7 Croham Valley Road.

3.10 The following applications at nearby sites are also of relevance:

*10 Croham Valley Road (diagonally opposite the application site)*

16/04907/OUT: Demolition of existing dwelling, erection of a building comprising 8 two bedroom flats, formation of vehicular access and provision of parking area with 8 spaces, cycle and refuse stores.

**Permission granted**

17/06096/RSM: Reserved matters associated with the above application

**Approved** The development has now been substantially completed

3.11 Three further schemes are currently under consideration and are listed below:

*3 Croham Valley Road (next door property)*

18/06067/FUL Demolition of existing building and the erection of a five storey building (two storey fronting Ballards Farm Road with basement levels) comprising 7 flats with creation of new access off Croham Valley Road, parking areas (7 spaces) refuse storage and landscaping

Application under consideration

*6 Croham Valley Road*

19/05034/FUL Demolition of existing house; erection of a two storey building plus roof space to provide 9 apartments; provision of 8 car parking spaces, refuse store and new landscaping.

A report into this proposal appears elsewhere on this agenda.

*R/O 31-33 Croham Valley Road*

19/04615/FUL) Erection of 2 two storey detached buildings with accommodation within the roof-space comprising 8 flats, bin store, the formation of vehicular access and provision of 7 parking spaces.

A report into this proposal also appears elsewhere on this agenda.

#### **4 LOCAL REPRESENTATION**

4.1 The application has been publicised by way of letters sent to the occupiers of 6 adjoining properties around the application site. The number of representations received from neighbours in response to notification and publicity of the application were as follows:

No of individual responses: 33      Objecting: 33

No of petitions received: 0

4.2 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Summary of objections	Response
<b>Density of Development – and Quality of Accommodation</b>	
<p>Gross overdevelopment of the site Cramped development</p> <p>Inadequate amenity space for future occupiers</p> <p>Schemes proposed at 3, 5, 6, 8 and 10 Croham Valley Road – possibly 150 new residents and their cars replacing 5 houses. Cumulative impact needs to be considered</p>	<p>The proposed density of development would equate to around 303 habitable rooms per hectare and 67 units per hectare. Whilst the habitable rooms per hectare figure would marginally exceed the range specified in the London Plan, the units per hectare would comply with the suggested range (albeit at the top of the range)</p> <p>All future gardens/amenity spaces would comply with required space standards</p> <p>Officers are satisfied that cumulative impact has been properly considered (and will continue to be considered). Further commentary is included below.</p>
<b>Character and Appearance</b>	
<p>Mass, depth and height would be overbearing on 7 Croham Valley Road.</p> <p>Significantly larger than neighbouring houses</p> <p>Bulk, mass and design out of keeping and will cause harm to local character. Design is just plain ugly. The scale of development means that the neighbouring property would be dwarfed –cutting out the only sunlight captured by the rear garden to 7 Croham Valley Road. Bins, bikes parking spaces not in character with the appearance of Croham Valley Road and will add clutter to the Croham Valley Road frontage.</p> <p>Terraced houses are not a feature of the area</p>	<p>Officers are satisfied that the scale and mass reflects the policy approach – and seeks to identify with the character of neighbouring properties – especially as the form, character and appearance of neighbouring properties is quite varied. Further commentary is included in later paragraphs.</p>



<p>Ballards Farm Road is a semi-rural lane well used by local people for walking, jogging, cycling and horse riding. The proposal will cause harm to the rural nature of the area</p> <p>Ridge line higher than adjacent properties on both roads</p>	<p>Ballards Farm Road is used by vehicular traffic – providing access and the existing house is accessed off Ballards Farm Road.</p> <p>The proposed building (fronting onto Croham Valley Road) has a higher ridge and eaves height (compared to 7 Croham Valley Road) this does not necessarily render the scheme unacceptable.</p>
<p><b>Neighbour Amenity</b></p>	
<p>Loss of privacy</p> <p>Query over the boundary position between the site and 7 Croham Valley Road – with the development affecting the existing boundary condition – detrimental to the neighbour’s residential amenity.</p>	<p>The degree of overlooking would be within acceptable limits, in view of the changes in topography and the angled nature of surrounding properties.</p> <p>The immediate neighbour raised concern about the future of an existing Laurel hedge, in place on the boundary with 5 and 7 Croham Valley Road. The application has subsequently been modified – setting back a proposed retaining wall off this boundary. Officers are now satisfied that there should be sufficient growing medium to ensure that the Laurel hedge is suitably protected and maintained in the future. Planning conditions are recommended to deliver high quality landscaping along with the retention of the Laurel hedge.</p> <p>The proposed building would be sited north of due west and therefore whilst it is appreciated that there will be a loss of late afternoon sun into the rear garden attached to 7 Croham Valley Road, BRE Guidance advises that under such circumstances, sunlight cannot be relied upon.</p>
<p><b>Highway Impacts and Car Parking</b></p>	

<p>Ballards Farm Road is a private road and the development would increase costs for owners of the road.</p> <p>Inadequate space for turning Inadequate parking provision resulting in increased on-street parking pressure</p> <p>Vehicles manoeuvring and parking in Croham Valley Road will cause serious highway danger Vehicles parked on Croham Valley Road will impede the safe passage for buses</p> <p>No scope of overspill parking on Ballards Farm Road</p> <p>Ballards Farm Road is not suitable for extra traffic as it's a single track lane</p> <p>How will construction vehicles access Ballards Farm Road without causing damage?</p>	<p>There are no changes to existing access arrangements off Ballards Farm Road and officers are satisfied that turning on site (following submission of amended drawings) is adequate to allow vehicles to exit parking areas in forward gear</p> <p>The level of car parking (66%) might well lead to some overspill onto the public highway – although officers are satisfied that there is spare capacity on street (excluding Ballards Farm Road and Croham Valley Road) to manage this effect (even after taking into account cumulative impact). It is recommended that contributions be made to ensure that any highway impacts are satisfactorily managed, alongside a contribution towards sustainable transport measures (including opportunities for a localised car club operation).</p> <p>The scheme would be subject to the agreement of a construction logistics plan – which will manage how demolition, ensuring that construction activities can safely take place on site – with areas set aside for delivery of materials to site (including loading and unloading) and a secure site compound.</p>
<p><b>Impact of Excavation</b></p>	
<p>Significant excavation required, concern over flooding, vegetation and ecological impact</p>	<p>The level of excavation is likely to be relatively extensive (especially towards the bottom of the site – fronting onto Croham Valley Road). The scheme would be expected to accommodate sustainable drainage measures (with water attenuation likely within car parking areas) in order to manage any drainage impacts.</p>

<p>Significant level changes and retaining walls will increase flood risk</p> <p>Considerable excavation is required which led to concerns regarding land stability</p>	<p>Details of any retaining walls and the effect of the scale of excavation would be a matter for the developer to satisfy neighbours (through party wall negotiations) – with such matters being a private issue between neighbouring landowners.</p> <p>The proposals have been supported by a Flood Risk Assessment and mitigation (in terms of surface water drainage and ground water flood risk) has been covered through the use of planning conditions</p> <p>The site is not included in an area of importance for nature conservation with the site being a well maintained/manicured residential garden.</p>
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4.3 Croham Valley Resident’s Association has objected to the proposal (referring the matter to Planning Committee). Their concerns are listed below:

- Significant increase in the scale of development and in terms of occupancy, is likely to be higher compared to the previously refused scheme
- Scheme would have an over-bearing impact on the neighbouring occupier at 7 Croham Valley Road
- The proposed development would be totally out of character with the immediate areas – characterised by detached houses –with the eaves and ridge heights being substantially higher than neighbouring houses
- Lack of car parking. There should be space for 6 parking spaces on site as a minimum – and there will be overspill parking onto Croham valley Road which is on a bus route
- Loss of privacy with overlooking to neighbouring properties
- Need to take into account other proposed developments in the area and especially the proposals for 3 Croham Valley Road
- Extent of excavation and construction of retaining walls which will significantly affect the water table and surface water flooding

4.4 Councillor Neal has objected to the scheme (referring the matter to Planning Committee) raising the following issues:

- Overdevelopment, out of character in size and design
- Inadequate parking
- Overbearing to other residents

## **5 RELEVANT PLANNING POLICIES AND GUIDANCE**

5.1 In determining any planning application, the Council is required to have regard to the provisions of its development plan so far as is material to the application and to any other material considerations; the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted development plan consists of the Consolidated London Plan 2015, the Croydon Local Plan 2018 (CLP) and the South London Waste Plan 2012.

5.2 Government Guidance is contained in the National Planning Policy Framework (2019), The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Requiring good design.
- Permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions

5.3 The main policy considerations raised by the application are:

### Consolidated London Plan 2015 (LP):

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice
- 3.9 Mixed and balanced communities
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.13 Sustainable drainage
- 6.9 Cycling
- 6.11 Smoothing traffic flow and tackling congestion
- 6.13 Parking
- 7.2 Designing out crime
- 7.4 Local Character
- 7.6 Architecture
- 7.14 Improving air quality
- 7.19 Biodiversity and access to nature
- 7.21 Trees and woodland

### Emerging New London Plan

5.4 Whilst the emerging New London Plan is a material consideration, the weight to be afforded is down to the decision maker, linked to the stage the Plan has reached in

its development. The Plan appears to be close to adoption. The Mayor's Intend to Publish version of the New London Plan is currently with the Secretary of State and at the time of drafting this report, no response had been submitted to the Mayor. Therefore, the New London Plan's weight has increased following on from the publication of the Panel Report and the London Mayor's publication of the Intend to Publish New London Plan. The Planning Inspectors' Panel Report accepted the need for London to deliver 66,000 new homes per annum (significantly higher than existing adopted targets) but questioned the London Plan's ability to deliver the level of housing predicted on "small sites" with insufficient evidence having been presented to the Examination to give confidence that the targets were realistic and/or achievable. This conclusion resulted in the Panel Report recommending a reduction in London's and Croydon's "small sites" target.

- 5.5 The Intend to Publish version of the New London Plan has accepted the reduction of Croydon's overall 10 year net housing figures - from 29,490 to 20,790 homes, with the "small sites" target reduced from 15,110 to 6,470 homes. Crucially, the lower windfall housing target for Croydon (641 homes a year) is not dissimilar to but slightly higher than the current adopted 2018 Croydon Local Plan target of 592 homes set for windfall sites each year.
- 5.6 Should the Secretary of State support the Intend to Publish New London Plan, the overall housing target in the New London Plan would be 2,079 new homes per annum (2019 – 2029) compared with 1,645 in the Croydon Local Plan 2018. Therefore, even with the possible reduction in the overall New London Plan housing targets (assuming it is adopted) Croydon will be required to deliver more new homes than our current Croydon Local Plan 2018 and current London Plan (incorporating alterations 2016) targets.
- 5.7 For clarity, the Croydon Local Plan 2018, current London Plan (incorporating alterations 2016) and South London Waste Plan 2012 remain the primary consideration when determining planning applications.

Croydon Local Plan 2018 (CLP 2018):

- SP2 on homes
- SP4 on urban design and local character
- SP6 on environment and climate change
- SP8 on transport and communications
- DM1 on housing choice for sustainable communities
- DM10 on design and character
- DM13 on refuse and recycling
- DM16 on promoting healthy communities
- DM19 on promoting and protecting healthy communities
- DM23 on development and construction
- DM24 on land contamination
- DM25 on sustainable drainage systems and reducing flood risk

- DM27 on biodiversity
- DM28 on trees
- DM29 on promoting sustainable travel and reducing congestion
- DM30 on car and cycle parking in new development

5.8 The relevant Supplementary Planning Guidance is as follows:

- London Housing SPG (March 2016)
- The Nationally Described Space Standards (October 2015)
- Croydon Suburban Design Guide Supplementary Planning Document April 2019

## **6 MATERIAL PLANNING CONSIDERATIONS**

The main planning issues raised by the application that the Planning Committee is required to consider are as follows:

- Principle of development
- Townscape and visual impact
- Residential amenity
- Living conditions of future occupiers
- Parking and highway safety
- Flood risk and sustainability
- Trees and biodiversity
- Other planning matters

### **Principle of Development**

- 6.1 The London Plan and Croydon Local Plan identify appropriate use of land as a material consideration, to ensure that opportunities for development are recognised and housing supply optimised. It is acknowledged that windfall schemes, which provide sensitive renewal and intensification of existing residential areas, play an important role in meeting demand for housing across London, helping to address overcrowding and affordability issues. The impact of the emerging London Plan on this policy approach is set out above.
- 6.2 Policies seek to prevent the net loss of 3 bedroom homes (as originally built) or homes less than 130m<sup>2</sup> and ensure that 30% of new homes are suitably sized for families.
- 6.3 The site is located within an existing residential area. The application proposes two buildings (each containing 3 family houses) which would provide additional family homes within the borough, which the Council is seeking to promote. Therefore, whilst the existing house is protected, as a three-bed home there would be a significant uplift in family accommodation, helping meet the strategic objective of 30% of all new homes being for families.

- 6.4 A number of residents have argued that the proposed development would constitute over-development of the site – in excess of density guidelines. The London Plan density matrix (where suburban PTALs are in the region of 2-3) suggests a density range of 150-250 habitable rooms per hectare and 35-65 units per hectare. In this case, the density of development would equate to around 300 habitable rooms per hectare and 67 units per hectare. Whilst this exceeds density ranges (in terms of habitable rooms) it is very close to the top end of the range (in terms of number of units) and in any case, consideration of the density matrix over the years has become a lesser consideration with London Boroughs expected to thoroughly test the elements that help inform the appropriateness of a particular density (neighbour impact, residential quality - including landscaping and amenity space, highway considerations, car parking and design quality). In any case, the emerging London Plan, which is a material consideration, is seeking to remove reference to the density matrix (which was accepted by the Panel Report – mentioned earlier in this report). In short, officers feel that this proposal sits well within its context and properly mitigates the various impacts.

### **Townscape and Visual Impact**

- 6.5 This part of Croham Valley Road is mostly characterised by large detached dwellings with traditional forms of two storeys with pitched roofs. In more recent times, infill has occurred on the northern side of Croham Valley Road. More recently, a flatted development (10 Croham Valley Road – LBC Ref 17/06096/RSM) has received planning permission and is nearing completion.
- 6.6 Ballards Farm Road differs in character from Croham Valley Road; being a single track access lane with a far more rural appearance. The road has landscaped verges with no dedicated footpaths with heavily treed/landscaped site boundaries. There are detached outbuildings to the front of the buildings/dwellings with some detached garages. Whilst these structures are traditionally designed, they all have differing appearances and relationships with the public realm.
- 6.7 Croydon Local Plan (CLP) (Policy DM10.1) states that proposals should seek to achieve a minimum height of 3 storeys. The Suburban Design Guide (SDG) suggests appropriate ways of accommodating intensified use of sites in different scenarios. Where surrounding buildings are predominantly detached dwellings of two (2) or more storeys, new developments may be three (3) storeys with an additional floor contained within the roof space or set back from the building envelope below.



Figure 2.10c: Where surrounding buildings are predominantly detached dwellings of two (2) or more storeys, new developments may be three (3) storeys with an additional floor contained within the roof space or set back from the building envelope below.

- 6.8 In accordance with the abovementioned policy and guidance, the proposed building fronting onto Croham Valley Road comprises three full storeys (one partly set below the level of the highway) with accommodation contained in the roof space. Whilst it is accepted that the scheme would be a larger development compared to the adjacent dwelling at 7 Croham Valley Road (with eaves and ridge lines being higher) the proposed building would have a traditional form with a half hipped and pitched roofs, respecting the general character of built form in the area. By reason of the careful levelling of the building, the eaves height of the proposed new building would be approximately 0.5 metres above the neighbouring eaves height (7 Croham Valley Road) and an increase of around 1 metre at ridge height. Officers are satisfied that this variation is acceptable in this particular instance bearing in mind the general variation in built form. The SDG also advises that developments need not necessarily step down in height towards neighbouring buildings of a lesser height.
- 6.9 Whilst it is appreciated that the scheme proposes a residential terrace rather than a single detached house, the footprint of the composite terrace would be similar to the building footplates found elsewhere in Croham Valley Road. The scheme works and responds effectively to the changing ground levels – albeit with the need for retaining walls and a terraced rear garden arrangement (similar to the arrangements found at 7 Croham Valley Road). Landscaping, boundary treatment and the details of retaining walls would need to be considered in detail as part of the planning conditions discharge process – to ensure that the scheme responds well to its surrounding context.
- 6.10 The western side element would be visible within the street scene along Croham Valley Road. The building would have greater depth (compared to 7 Croham Valley Road) but would only exceed the depth of this neighbour by approximately 3.5 metres (taken from the neighbouring rear garage wall) and 2.5 metres from the main rear elevation of this neighbouring house. The planning application for 3 Croham Valley Road remains under consideration although officers are comfortable with the appearance of the side flank elevation of the proposed development which in any case, would be partially masked should redevelopment of 3 Croham Valley Road comes forward in the future. Overall, the building would not appear overly deep or bulky.



- 6.11 The building is proposed to have traditional external materials with stock brick under a plain tiled roof. The proposed materials are commonly found in the area and should help the development to respect its setting and surroundings. The scheme would involve extensive excavation at the foot of the slope with terraced gardens to the rear which mirrors the arrangement in place at 7 Croham Valley Road (which arose out of the previous subdivision of 7 Ballards Farm Road a few years ago)
- 6.12 A vehicle crossover and elements of hardstanding would be provided – accessed off Croham Valley Road. Access driveways and forecourt parking are features found along Croham Valley Road and there is no objection to these interventions from a visual viewpoint. Sufficient space should be accommodated on site for two car parking spaces (with easy manoeuvring on site) and space made available for bikes and bin storage, replacement tree planting and visibility splays. Space would be available within this area to accommodate these required facilities without resulting in an overly cluttered appearance.



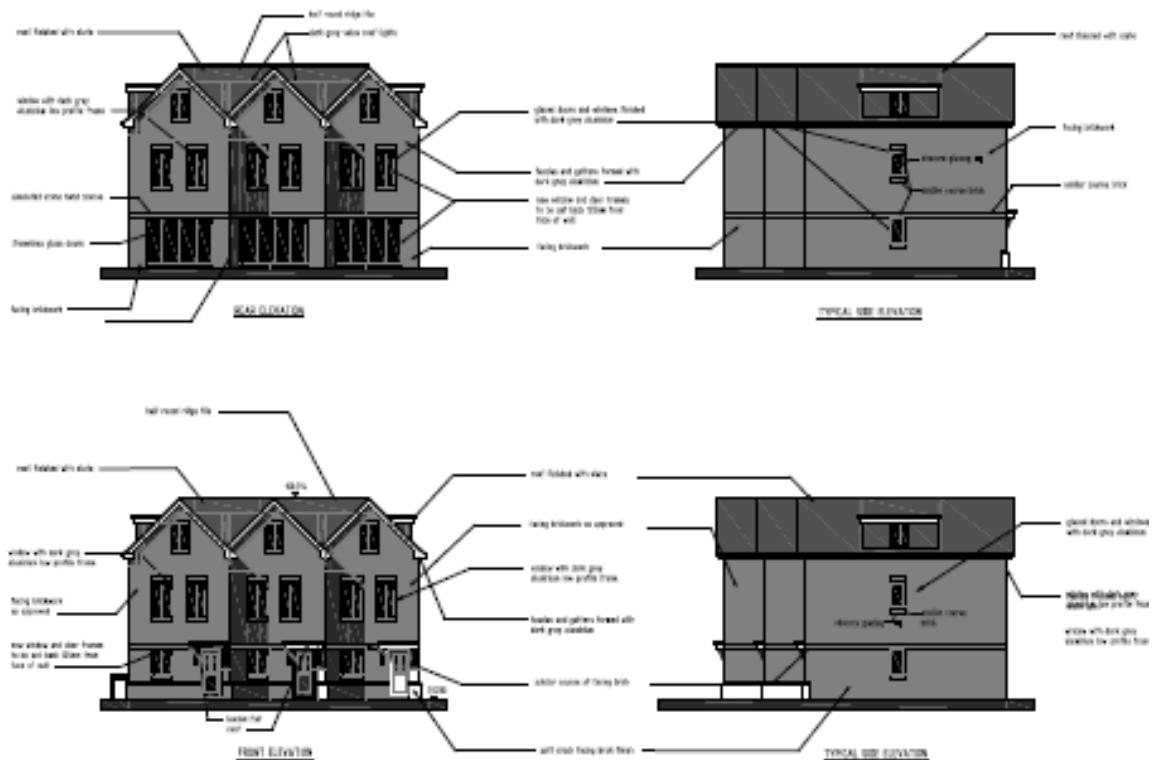
*Figure 4 Elevations of Croham Valley Road Frontage Building and CGI*

- 6.13 The same policy and guidance within the SDG apply would apply to the proposed development fronting onto Ballards Farm Road. The scale of development (being located at the top of the slope and on more level ground) would present itself as a part two/part 3 storey terrace with accommodation in the roof void with side dormers and gable features. Policy DM10 advises that development within rear gardens should be subservient to the existing property on site and this approach is followed through in the Suburban Design Guide. Officers feel that this approach is not especially relevant in this particular case as the existing house (situated at the top

of the slope) is to be demolished and replacement buildings situated at the top of the slope are always going to be more dominant (when viewed from the rear gardens of properties situated further down the slope). The terrace would also be slightly staggered to provide some individuality to the terraced arrangement. Given the policy requirement and intensification agenda, the mass of the development would suitably correspond to the surrounding area. Adequate gaps would be maintained to the boundaries with adjacent properties.

6.14 The building would have a brick finish as per the frontage building, but would have a slate roof and dark grey windows of a more contemporary appearance. The detailed design and architecture would be acceptable in this context, with materials suitable and respectful of the surrounding area. Changes to existing access arrangements would be minimal and following some modification to on site car parking arrangements (including a reduction in the number of on-site car parking) the layout would now allow for vehicles to turn on site and exit onto Ballards Farm Road in forward gear. Again, sufficient space would be made available for bike and refuse storage/collection.

Figure 5 Proposed Elevations – Terrace fronting onto Ballards Farm Road



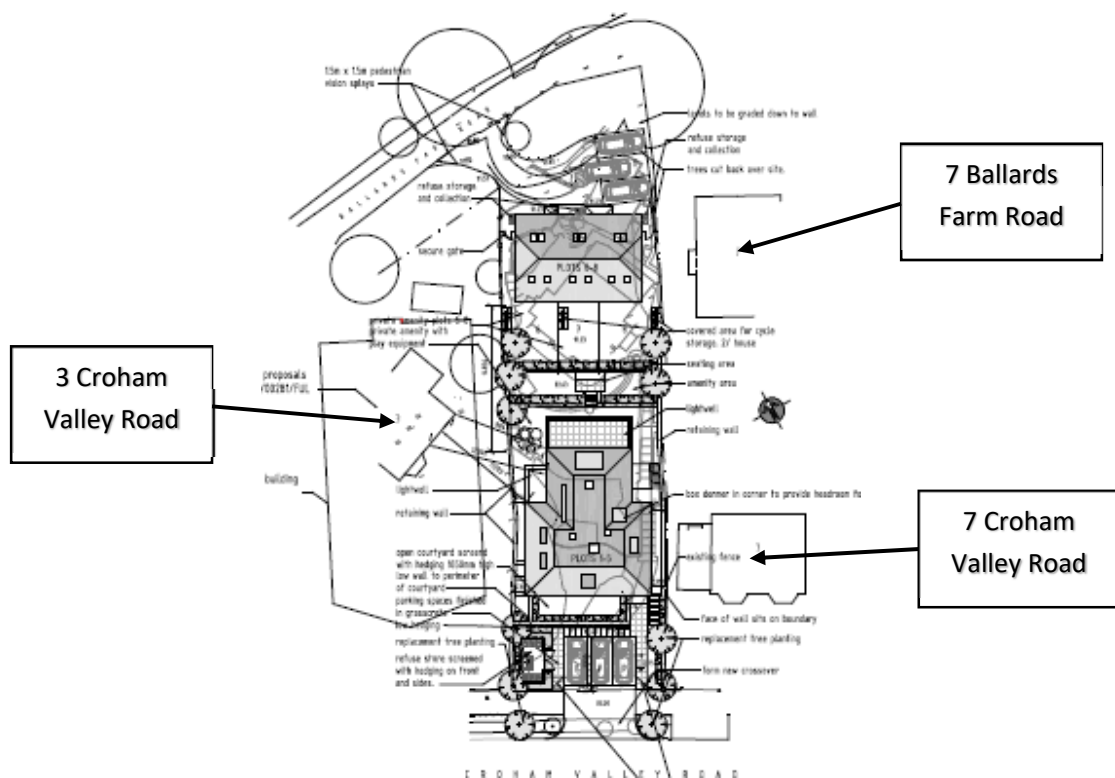
6.15 Overall, having considered all of the above, against the backdrop of housing need, the proposed development would comply with the objectives of the above policies in terms of respecting local character.

## Impact on Neighbouring Residential Amenity

6.16 The properties most affected by the development would be the immediate neighbours (3 and 7 Croham Valley Road and 7 Ballards Farm Road).

### 3 Croham Valley Road

6.17 3 Croham Valley Road is situated to the north-west of the application site, towards the middle of the plot, and the dwelling is angled so that the rear windows face into the garden that slopes down towards Croham Valley Road. The ridge height of the proposed houses fronting onto Croham Valley Road would roughly align with the eaves line of this neighbouring property and as such, the proposed rear facing windows would be at a lower level and angled away from the rear elevation of this neighbouring property. The windows in the side elevation of the proposed building are shown to be obscurely glazed and are also at an angle to the neighbouring house.



6.18 Whilst the building would be readily visible from and impact on views enjoyed by the residents of 3 Croham Valley Road (with a minimum separation of 10 metres) given the change in levels and angled relationship, the proposed building would not be

unduly intrusive or cause any harmful loss of light. 3 Croham Valley Road would still enjoy an open outlook to the south.

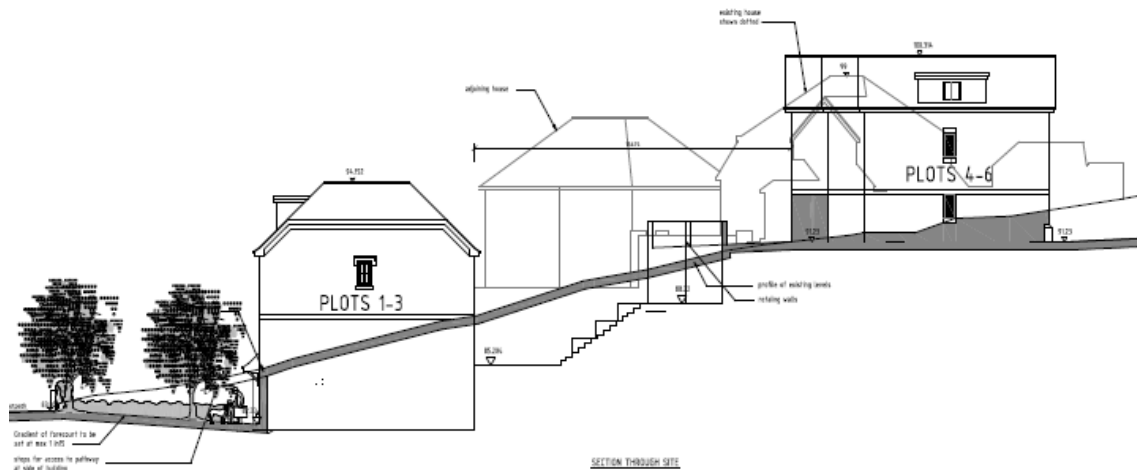
- 6.19 The terrace fronting onto Ballards Farm Road would be located adjacent to the parking area (attached to 3 Croham Valley Road) and would be separated by established boundary planting. The proposed building would be over 10 metres from the closest point to 3 Croham Valley Road and would be situated (again) at an oblique angle. Windows in the side elevations of the proposed building would again serve bathrooms and would be obscurely glazed and should therefore respect privacy. The rear/south facing windows would again be positioned at an oblique angle to 3 Croham Valley Road. Some windows are located in the side elevation of 3 Croham Valley Road. The SDG sets out that little protection is afforded to side facing windows in these circumstances. Given the orientation of the buildings and angled relationship/gap, the proposal would respect light and outlook.
- 6.20 A planning application (LBC Ref 18/06067/FUL) has been submitted (yet to be determined) for the redevelopment of 3 Croham Valley Road involving the demolition of the house and redevelopment to form 8 flats. The proposed development proposes a contemporary response to appearance and layout, with the development cascading down the slope. All proposed units would enjoy a southerly aspect with no proposed side windows. Officers are satisfied that the current proposal for 5 Croham Valley Road would not prejudice the redevelopment potential of 3 Croham Valley Road and any subsequent determination of the associated planning application. It is hoped that the proposals for 3 Croham Valley Road will be ready to present to Planning Committee in the next month or so. .

### 7 Croham Valley Road

- 6.21 This dwelling is an infill development that was granted planning permission in 2015 (on appeal). It has an attached garage situated adjacent to the boundary with a bathroom above. The previous 2019 refused scheme raised significant concerns around impact on this immediate neighbour; the previously proposed block of flats were shown to project 7 metres beyond the rear elevation of this neighbouring property which would have been overly intrusive and over-bearing.
- 6.22 The current proposal seeks to deal with this previous issue and in comparison to the previously refused scheme, the proposed building has now been reduced in depth (by 4.7 metres). However, following on from the removal of the previously stepped projection and the introduction of a revised built form, the north eastern corner of the proposed building would now be sited slightly closer to 7 Croham Valley Road. That said, the proposed building would not encroach into the 45 degree angle (highlighted in the SPD) taken from any main habitable window. Moreover, the relationship with the bathroom window of 7 Croham Valley Road (over the side garage) would also be acceptable. The ground floor level to 7 Croham Valley Road, is set down into the rear garden with the patio already enclosed by a relatively high retaining wall which in itself, encloses the rear patio and restricts light from entering the ground floor rooms (which in any event is dual aspect). Consequently, given the

significant reduction in depth and taking into account SPD advice, the amenities of this immediate neighbour (in terms of daylight, sunlight, outlook and enclosure) would be acceptable.

- 6.23 The rear garden (associated with to 7 Croham Valley Road) is tiered – with an overall depth of around 7 metres. Existing ground floor accommodation has only limited outlook (in view of the change in level within the rear garden) with steps leading to the grassed upper terrace, which is the only area which receives mid/late afternoon sun. The proposed building would extend approximately 2.5 metres beyond the rear elevation of this neighbouring property and part of the upper level and roof would be visible from the garden



*Figure 6 Section through the Site – Viewed from 7 Croham Valley Road and 7 Ballards Farm Road*

- 6.24 This neighbour has raised a number of concerns as regards the proposed development but specifically the extent of excavation required close to their boundary and the details of any retaining wall between the properties; concerns over the future of an existing Laurel hedge which currently forms a verdant boundary between the two properties. In response to these concerns, the scheme has been modified by setting back the retaining wall off the existing site boundary, which should provide sufficient space for the existing hedge to flourish – whilst still providing a landscaped boundary between the two properties. A small portion of the retaining wall might well be visible (albeit set back from the common boundary and behind the existing Laurel hedge) but officers are satisfied that the amendment helps to overcome this valid concern.

- 6.25 The terrace fronting onto Ballards Farm Road would be located over 20m from the rear elevation of 7 Croham Valley Road and given the site layouts, levels, orientation and distance, this element of the proposal would not cause any harmful loss of light, outlook or privacy.
- 6.26 The owners of 7 Croham Valley Road dispute the location of the eastern side boundary as shown on the submitted plans; arguing that the boundary does not align with the boundary that was granted planning permission back in 2015 or the subsequent plans submitted as part of the subsequent conditions discharge process. The applicant has been made aware of this apparent disparity and apart from modifying the location of the retaining wall, has not amended the plans and/or served notice on the neighbouring owners. That said, this remains a private matter between parties and does not represent a reason to refuse planning permission.

#### 7 Ballards Farm Road

- 6.27 This property is located to the north east of the site and has a number of side facing windows (lighting day rooms, bedrooms, an en-suite bathroom and a study) with a heavy tree screen along the western boundary. The existing dwelling and garage is already sited in close proximity to the site boundary.
- 6.28 Whilst at some points, the proposed terrace would be closer to the boundary compared to the existing building, given the tree screen, it is not considered that the proposed building would cause any harmful loss of light or outlook. Apart from the study window, all other side windows are secondary (with rooms having dual aspect) or serve bathrooms and the proposed building would not extend beyond the front or rear elevation of this neighbouring property. The proposed side windows would serve bathrooms or stair cores and would be conditioned to be obscure-glazed should planning permission be forthcoming.
- 6.29 The rear most element of the proposed block fronting Croham Valley Road would be 15 metres from the rear elevation of 7 Ballards Farm Road but would be situated at a significantly lower ground level – which would provide appropriate levels of mitigation.
- 6.30 In view of the sloping topography and the challenging rear garden levels, it is recommended that permitted development be limited to allow the local planning authority to further consider the effect of householder extensions and any rear outbuildings.

#### **The Standard of Accommodation for Future Occupiers**

- 6.31 All of the proposed residential units would meet the Nationally Described Space Standards (NDSS).
- 6.32 The units in the block facing onto Croham Valley Road would be single aspect (at lower ground floor level) due to the levels of the site and the desire to limit the extent of excavation. However, all of the upper floors of the building would benefit from

dual or triple aspect and would therefore have a satisfactory outlook. The lower-ground floor is shown as a bedroom with a front facing window. There would be some space available in the front forecourt to provide some defensible planting – to maintain some degree of privacy for these bedrooms.

- 6.33 All houses would benefit from private gardens to the rear. In view of the sloping nature of the site, the gardens would have to be terraced with an initial level area leading from the upper ground floor level – stepping up to a higher level garden area, enclosed by the retaining wall (highlighted above). Overall, there would be sufficient useable space within the garden. In addition, the stepping of the garden levels and the height of the retaining walls in relation to the ground floor windows of the houses would be sufficiently shallow/low to ensure that the outlook would not be unduly obstructed.
- 6.34 The terraced dwellings to the north of the site would be either dual or triple aspect with private rear gardens of adequate size and privacy. Overall, all of the proposed units would be of an acceptable quality and would benefit from sufficient private amenity space. In terms of accessibility, the block fronting Croham Valley Road could provide level access through the front door.
- 6.35 London Plan Policy 3.8 'Housing Choice' requires 90% of dwellings to meet M4(2) 'accessible and adaptable dwellings' building regulations requirement. Full compliance with this can be difficult to secure with steeply sloping sites and the London Plan does recognise that full compliance can be difficult to secure in various circumstances. In this case, the houses proposed onto Croham Valley Road have the option to install a lift (between the lower ground floor bedrooms and the upper floor main reception room – to enable a wheelchair occupier to move between the lower ground and upper ground levels – and into the lower terraces of the rear garden areas. This would allow for some elements of Part M4(2). Similarly, the houses fronting onto Ballards Farm Road would have level access at ground floor level – with a reasonably sized ground floor WC and access into the rear garden which should be sufficient to comply with M4(1).
- 6.36 Overall, officers are satisfied that the proposals would result in a good standard of accommodation for future occupiers.

### **Parking and Highways**

- 6.37 The site has a PTAL rating of 2 and is served by two bus services (64 and 433 bus routes); linking Thornton Heath Pond with New Addington via Selsdon and Central Croydon and linking Addington Village with East and West Croydon Stations.
- 6.38 The applicants have submitted a detailed transport statement alongside a parking stress survey which has considered overnight on-street parking availability – whilst taking into account cumulative impact should all current applications (highlighted above) be granted planning permission.

- 6.39 The London Plan sets out that the maximum car parking standards for residential developments – which are based on public transport accessibility levels and local character. It states that 1-2 bedroom properties should provide a maximum of up to 1 space per unit, with up to 1.5 spaces per unit being provided for 3 bedroom properties. Based upon this ratio and the unit mix proposed, a maximum of 8 parking spaces should be provided. However, using 2011 Census data the proposed development is likely to generate demand for around 5 car parking spaces.
- 6.40 Officers have reviewed all current Croham Valley Road applications and commented accordingly. Officers are satisfied that there is adequate capacity on street to accommodate any overspill car parking; up to 4 car parking spaces (as worst case) might need to be accommodated on-street and the applicant has identified spare capacity on Croham Road and Manor Way (in close proximity of the application site) – with spare capacity for around 30 cars. Officers concur that there is spare capacity. Other developments proposed in and around Croham Valley Road have on site car parking capacity (8 car parking spaces for 9 flats in the case of 6 Croham Valley Road and 7 car parking spaces for 8 flats in the case of land to the R/O 31-33 Croham Valley Road) and even with these other sites, there should be capacity for additional on-street car parking should the need arise.
- 6.41 Policy SP8 and DM29 seeks to manage use of the private car and promote sustainable travel. DM30 requires a car club space to be provided on nine unit schemes, where there is likely to be interest from an operator. Whilst there is sufficient parking on site and on street to ensure that the impact on the network is satisfactory, sustainable travel should still be promoted in accordance with these policies. Therefore, a contribution is recommended, and has been agreed with the applicant, towards the provision of sustainable travel measures, most likely to be traffic management measures on Croham Valley Road (to ensure that the bus route is not affected by any overspill parking) and the provision of a car club space in the local area, which will help mitigate overspill parking as well as encouraging sustainable travel. Similar arrangements have been agreed with the developers of 6 Croham Valley Road and the R/O 31-33 Croham Valley Road. Taking into account the site's accessibility to public transport, relevant car ownership data and capacity for on-street parking to accommodate any overspill, the proposal would provide for an appropriate number of parking spaces which would not detrimentally impact highway safety within the surrounding area.
- 6.42 There is scope to provide the required visibility splays and vehicles should be able to turn on site and enter and exit the car parking bays (from both Croham Valley Road and Ballards Farm Road) in forward gear. Moreover, the degree of change at the entrance of Ballards Farm Road would be minimal. There are some staked street trees/bushes (in Croham Valley Road) that would need to be removed and replaced and this could be agreed when the crossover works are submitted for consideration by the highway authority. The costs of tree replacement would be secured through the S.106 Agreement. The change in the number of vehicles using Ballards Farm Road would be relatively minimal and existing highway conditions would be suitably maintained.



- 6.43 It is recommended that a Demolition, Construction Logistics and Environmental Management Plan will be required by a condition before commencement of work, given the site's location in a residential area and to deliver a managed solution to highway impact associated with the demolition and construction process.

### **Cycle and Refuse Storage**

- 6.44 The proposed cycle stores are proposed within the forecourt areas along with refuse storage areas. The location of these facilities are considered acceptable (for storage and collection) although detailed design of the various enclosures would need to be controlled and approved through the use of planning conditions to be discharged at a later date.

### **Trees and Biodiversity**

- 6.45 The site is not covered by any tree preservation orders and there are no trees of any particular merit on the site although the importance of the existing hedge boundary is acknowledged. The plans show some replacement landscaping although full details of hard and soft landscaping, including boundary treatment would be secured by condition should planning permission be forthcoming. There is a protected tree within the front garden of 7 Ballards Farm Road – but the proposed development would not interfere with this protected tree.
- 6.46 As raised above, there is an immature street tree to the front of the site on Croham Valley Road and some shrubs that would need to be removed to facilitate the new vehicular access. Due to the young age of the tree currently in situ the Trees and Woodland Officer has no objection to its removal, provided at least another two street trees of a suitable approved species are planted post development works and the costs of supply, planting and maintaining (regular watering, weeding and tie adjustments) for a minimum two years post works are fully covered by the developer. There is no objection to the removal of the shrubs. These arrangements can be agreed with the highway authority when the crossover is installed.
- 6.47 Whilst there is no reason to believe that any protected species would be affected by the proposed development, an informative should be included on any decision making the applicant aware that it is an offence to harm protected species or their habitat and in the event that protected species are found on site the applicant should refer to Natural England standing advice.

### **Environment and Sustainability**

- 6.48 Conditions can be attached to ensure that a 19% reduction in CO2 emissions over 2013 Building Regulations is achieved and mains water consumption would meet a target of 110 litres or less per head per day.
- 6.49 The site is located within an area some risk of surface water flooding and potential for groundwater flooding to occur. Policy DM25 requires all development to incorporate sustainable drainage measures (SuDS). A flood risk assessment was

submitted as part of the planning application and in many ways the re-profiling of the land/terracing might well allow for reduced surface water flows and might well provide capacity for water attenuation and sustainable urban drainage measures (especially within the car parking and garden areas). Given the location of part of the development at the bottom of a slope, partially dug in, there is some risk from groundwater flooding. The proposed mitigation of raising floor levels by 0.15m internally and installing non-return valves and covers to air bricks would suitably ameliorate this risk and a condition is recommended to secure full details

- 6.50 A planning condition is recommended requiring site specific SuDS measures as well as groundwater flood risk resistance and resilience measures.

### **Other Planning Matters**

- 6.51 The development would be liable for a charge under the Community Infrastructure Levy (CIL). This payment would contribute to delivering infrastructure to support the development of the area, such as local schools.

### **Conclusions and the Planning Balance**

- 6.52 This scheme seeks to make optimum use of the site, delivering a net gain in the number of family houses – contributing not only to housing targets but also the strategic 30% target for new family accommodation. The PTAL is relatively low and there is likely to be some pressure being placed on existing on street car parking conditions, but there is capacity on street to accommodate additional car parking demand associated with this development and other schemes currently in the development pipeline. The developer has accepted the need to work with the Council to encourage more sustainable forms of transport – as an alternative to the private car. The appearance of the site and the immediate area will change and evolve, but there is nothing intrinsically wrong with the form of development proposed, with buildings proposed fronting onto Ballards Farm Road as well as Croham Valley Road. The proposed houses would all comply with Nationally Prescribed Housing Standards and the impact of development on immediate neighbours would be acceptable

- 5.53 Taking all of the above planning considerations into account, the application is recommended for approval.